

RPD
LOT 1 ON RP617280
AREA: 5984m²

LOT 2 ON RP617280
AREA: 1.363ha

LGA: ROCKHAMPTON REGIONAL COUNCIL



LEGEND:

--- PROPERTY BOUNDARY



STAGE 1 - INTERNAL WORKS (Q1 & Q2 2019 APPROX):
INTERNAL EARTHWORKS, DRAINAGE AND PAVEMENTS.
EXTERNAL TABLE DRAIN REPROFILE



STAGE 2 - EXTERNAL WORKS (Q2 & Q3 2019 APPROX):
ROADWORKS, EXTERNAL DRAINAGE, WATER AND SEWER CONNECTION LINES.



STAGE 3 - INTERNAL BUILDING WORKS (Q3 2019 APPROX):
SIGNAGE, SHOP, CANOPIES, FUEL INSTALLATION AND SERVICES.

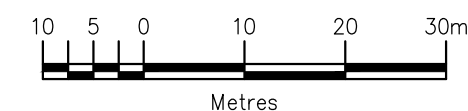
ROCKHAMPTON REGIONAL COUNCIL

APPROVED PLANS

These plans are approved subject to the current conditions of approval associated with

Development Permit No.: D/12-2021

Dated: 6 April 2021







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© Copyright TFA Group Pty Ltd.						1		06.12.18		LS		ISSUED FOR TENDER				PROPOSED SERVICE STATION CNR CAPRICORN HWY & HALL ROAD GRACEMERE, QLD.		SITE STAGING WORKS PLAN		TENDER	
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																				APPROVED 	
																				DATE CREATED 02.10.18	
																				A1 SCALE 1:375	
																				A3 SCALE 1:750	
																				DRAWING NO	
																				REV	
																				16273-C00	
																				1	

1. ALL DIMENSIONS AND CO-ORDINATES ARE TO BE VERIFIED ON SITE BEFORE WORK COMMENCES.
2. FIGURED DIMENSIONS ARE TO TAKE PRECEDENCE OVER SCALE READINGS.
3. ~~DURING CONSTRUCTION, BARRIERS, LIGHTS & SIGNS SHALL BE MAINTAINED TO ENSURE SAFE PASSAGE OF TRAFFIC AND PEDESTRIANS IN ACCORDANCE WITH THE REQUIREMENTS OF CAPRICORN MUNICIPAL DEVELOPMENT GUIDELINES.~~
4. ~~ALL WORKMANSHIP AND MATERIALS SHALL BE IN ACCORDANCE WITH AUSTRALIAN STANDARDS BY LAWS AND ORDINANCES OF CAPRICORN MUNICIPAL DEVELOPMENT GUIDELINES, AND WORKPLACE HEALTH AND SAFETY REQUIREMENTS.~~
5. THE CONTRACTOR SHALL MAKE GOOD, AT HIS COST, ANY DAMAGE TO ANY SERVICE IF SUCH DAMAGE OCCURS AS A RESULT OF HIS OPERATIONS.
6. ALL DISTURBED VERGE AREAS MUST BE REINSTATED WITH TURF.
7. ALTER ANY SERVICES WHEN THE RELEVANT AUTHORITY OR ASSESSMENT MANAGER DETERMINES THAT THE WORK ASSOCIATED WITH THIS DEVELOPMENT HAVE IMPACTED EXISTING SERVICES.
8. THE SURVEY INFORMATION SHOWN MAY NOT ADEQUATELY REFLECT CURRENT SITE CONDITIONS. THE CONTRACTOR SHALL ASSESS SITE CONDITIONS PRIOR TO COMMENCING CONSTRUCTION. ANY DISCREPANCIES IN THE SURVEY INFORMATION SHALL BE REPORTED TO THE SUPERINTENDENT. REFER TO SURVEY FOR DETAILS.
5. EXISTING SERVICES SHOWN HAVE NOT BEEN CONFIRMED ON SITE. THE CONTRACTOR SHALL DETERMINE THE LOCATION AND DEPTH OF SERVICES WITHIN OR ADJACENT TO THE WORKS BY CONTACTING THE RELEVANT SERVICE AUTHORITY AND APPROVED UNDERGROUND SERVICES LOCATOR A MINIMUM OF 14 DAYS PRIOR TO COMMENCEMENT OF WORKS. ADVISE THE SUPERINTENDENT OF ANY DISCREPANCIES AND CLASHES BEFORE PROCEEDING. THE CONTRACTOR SHALL BE REQUIRED TO COORDINATE AND PROGRAM WORKS TO RESOLVE ANY CLASHES WITH RELEVANT SERVICE AUTHORITIES PRIOR TO COMMENCEMENT OF ANY WORKS. RESOLUTION OF CLASHES SHALL BE APPROVED BY THE RELEVANT SERVICE AUTHORITIES.
6. ALL NEW WORK SHALL BE JOINED NEATLY TO EXISTING. PROPOSED LEVELS FOR CONNECTING TO EXISTING WORKS MAY BE VARIED WHERE NECESSARY ON SITE BY THE SUPERINTENDENT TO ACHIEVE A SATISFACTORY SMOOTH FINISH.
7. THE CONTRACTOR IS RESPONSIBLE FOR THE REMOVAL AND DISPOSAL OF ALL RUBBISH AND SPOIL FROM THE SITE.

1. ALL DIMENSIONS ARE TO BE CHECKED ON-SITE BEFORE WORK COMMENCES
2. FIGURED DIMENSIONS ARE TO TAKE PRECEDENCE OVER SCALE READINGS.
3. DURING CONSTRUCTION, BARRIERS, LIGHTS & SIGNS SHALL BE MAINTAINED TO ENSURE SAFE PASSAGE OF TRAFFIC AND PEDESTRIANS IN ACCORDANCE WITH THE REQUIREMENTS OF THE CAPRICORN MUNICIPAL DEVELOPMENT GUIDELINES.
4. ALL WORKMANSHIP AND MATERIALS SHALL BE IN ACCORDANCE WITH S.A CODES & BY-LAWS AND ORDINANCES OF THE CAPRICORN MUNICIPAL DEVELOPMENT GUIDELINES, AND WORKPLACE HEALTH AND SAFETY REQUIREMENTS.
5. CONTRACTOR TO VERIFY ALL INVERT LEVELS, SURFACE LEVELS, COVER OVER DRAINAGE LINES, AND MINIMUM FALLS ARE CORRECT & OBTAINABLE PRIOR TO COMMENCEMENT OF WORKS.
6. PIPES Ø375 AND LARGER TO BE REINFORCED CONCRETE OR FRC CLASS `2' APPROVED SPIGOT AND SOCKET WITH RUBBER RING JOINTS U.N.O.
7. PIPES UP TO Ø300 SHALL BE SEWER GRADE PVC-U MINIMUM CLASS SN6 WITH SOLVENT WELDED JOINTS..
8. PIPES SHALL BE LAID AS PER TYPICAL PIPE TRENCH DETAIL AND IN ACCORDANCE WITH AS1289 5.2.1.
9. FOR TRENCHES NOT UNDER PAVEMENTS WHERE EXCAVATED MATERIAL MAY BE USED FOR BACKFILL, THE MATERIAL SHALL BE COMPACTED TO THE SAME DENSITY AS THE UNDISTURBED MATERIAL EITHER SIDE OF THE TRENCH.
10. WHERE TRENCHES ARE IN ROCK, THE PIPE SHALL BE BEDDED ON A MINIMUM OF 50mm CONCRETE BED (OR 75mm BED OF 12mm BLUE METAL) UNDER THE BARREL OF THE PIPE.
11. ENLARGERS, CONNECTORS AND JUNCTIONS TO BE PRECAST OR PROPRIETARY FITTINGS WHERE PIPES ARE LESS THAN Ø300.
12. CARE IS TO BE TAKEN WITH LEVELS OF STORMWATER LINES. GRADES SHOWN ARE NOT TO BE REDUCED WITHOUT APPROVAL.
13. GRATES AND COVERS SHALL CONFORM TO AS 3996.
14. ALL FINISHED SURFACE LEVELS SHOWN ON GULLY GRATES AND MANHOLES ARE LOCATED ON THE CAST IRON COVER UNLESS OTHERWISE SHOWN.
15. ON COMPLETION OF PIPE INSTALLATION ALL DISTURBED AREAS MUST BE REINSTATED TO "AS FOUND" CONDITION.
16. THE CONTRACTOR SHALL MAKE GOOD, AT HIS COST, ANY DAMAGE TO ANY SERVICE IF SUCH DAMAGE OCCURS AS A RESULT OF HIS OPERATIONS.
17. CONSTRUCT STORMWATER LINES, PITS AND MANHOLES IN ACCORDANCE WITH CAPRICORN MUNICIPAL STANDARD DRAWINGS & SPECIFICATIONS IF NOT OTHERWISE DETAILED ON THE DRAWINGS.
18. ALL PITS/MANHOLES DEEPER THAN 1000mm TO BE FIXED WITH GALV. STEEL PROPRIETARY STEP IRONS.
19. ALL UNDERGROUND SERVICES ARE TO BE PROVEN ONSITE AND THE CONTRACTOR TO LIAISE WITH RELEVANT AUTHORITIES REGARDING ANY IMPACTS PRIOR TO COMMENCEMENT OF ANY CONSTRUCTION WORKS..

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUPPLYING, ERECTING AND MAINTAINING EROSION AND SEDIMENT CONTROL DEVICES ON THE SITE AND ENTRY TO THE SITE TO COMPLY WITH ALL STATUTORY AND THE CAPRICORN MUNICIPAL DEVELOPMENT GUIDELINES REQUIREMENTS.
2. THIS MAY INVOLVE THE CONSTRUCTION OF SEDIMENT FENCES TO THE PERIMETER OF THE SITE TO PREVENT CONTAMINATED STORMWATER ENTERING WATERWAYS AND ALSO APPROPRIATE MEASURES TO PREVENT TRACKING OF MATERIALS ONTO PUBLIC ROADS. IN ADDITION THE CONTRACTOR SHALL TAKE ALL REASONABLE PRECAUTIONS TO ELIMINATE DUST NUISANCE FROM THE SITE.



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ROCKHAMPTON REGIONAL COUNCIL

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Dated: **6 April 2021**

RPD

LOT 1 ON RP617280

AREA: 5984m²

LOT 2 ON RP617280

AREA: 1.363ha

LGA: ROCKHAMPTON REGIONAL COUNCIL

LEGEND:

- PROPERTY BOUNDARY
- SW — PROPOSED STORMWATER
- — PROPOSED SWALE
- OW — PROPOSED OILY WATER
- RW — PROPOSED RAINWATER
- GP GULLY PIT
- EGP EXISTING GULLY PIT
- GP1 OILY WATER GULLY PIT (WATER SEALED)
- TD TRENCH DRAIN
- DP DOWN PIPE
- MH MANHOLE
- 9.99 PROPOSED SPOT HEIGHT
- * 9.99 EXISTING SPOT HEIGHT
- FFL 9.99 FINISHED FLOOR LEVEL
- ⊗ 9.99 FINISHED SURFACE LEVEL AT TOP OF TANK TURRET & FUEL FILL BOX IS TO INCLUDE AN ADDITIONAL 25mm COVE UP OVER 300mm TO PERIMETER FROM SURROUNDING SLAB LEVEL. TANK TURRET LOCATIONS TO BE CONFIRMED BY CONTRACTOR TO SUPERINTENDENT FOR CHECKING LEVEL PRIOR TO PAVEMENT CONSTRUCTION.

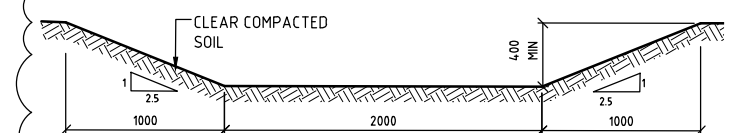


FOR SURVEY DATA REFER TO CONTOUR AND DETAIL SURVEY DRAWING (REF No R17005-1) PRODUCED BY HOFFMANN SURVEYORS DATED 23.02.2017.

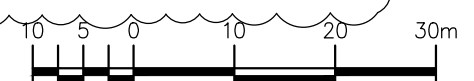
500mm x 75mm HIGH ROLLOVER BUND FORMED IN THE SLAB

PROPOSED FLOOD EMERGENCY FOOTPATH EXIT

STORMWATER INFRASTRUCTURE NUMBER TAG REFER TO STORMWATER PIT SCHEDULE FOR DETAILS



TYPICAL REPROFILED TABLE DRAIN

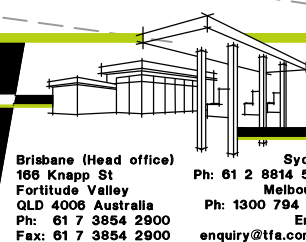


Metres

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REV	DATE	BY	DESCRIPTION
1	04.10.18	CV	ISSUED FOR TENDER
2	06.12.18	LS	RPEQ AND SURVEY DETAILS PROVIDED

PROJECT DETAILS
PROPOSED SERVICE STATION
CNR CAPRICORN HWY &
HALL ROAD
GRACEMERE, QLD.

DRAWING TITLE
SITE GRADING & DRAINAGE
PLAN - STAGE 1

STATUS			
TENDER			
DRAWN CV	APPROVED		
DATE CREATED 02.10.18	A1 SCALE 1:375	A3 SCALE 1:750	
DRAWING NO	REV		
16273-C02	2		

STORMWATER PIT/MANHOLE SCHEDULE					
STRUCTURE No	STRUCTURE DESCRIPTION	GRATE/LID TYPE	SL	IL US	IL DS
Out/A	4 x ø150 SLOPPING CONCRETE HEADWALL	N/A	16.710	FROM 1/A & 1/B: 16.350	16.300
1/A ✖	900 SQ CAST IN-SITU GULLY PIT WITH 200mm THICK WALLS & BASE	900 SQ CLASS 'D' GALV. OR C.I. GRATE & FRAME (HEELSAFE)	16.850	16.470	16.450
2/A ✖	600 SQ CAST IN-SITU GULLY PIT WITH 200mm THICK WALLS & BASE	600 SQ CLASS 'D' GALV. OR C.I. GRATE & FRAME (HEELSAFE)	17.500	17.205	16.790
3/A	ACO S200K POWER DRAIN (SK2-1 - SK2-19) (SLOPED CHANNEL)	CLASS 'D' INTERCEPT GRATE WITH POWERLOCK (HEEL SAFE)	17.500	17.300	17.205
1/B ✖	600 SQ CAST IN-SITU GULLY PIT WITH 200mm THICK WALLS & BASE	600 SQ CLASS 'B' GALV. OR C.I. GRATE & FRAME (HEELSAFE)	17.000	16.560	16.540
2/B	SPEL PURCEPTOR CLASS '1' MODEL P.040.1C.2C'	CLASS 'B' LIDS BY SPEL ENVIRONMENTAL	VARIES	16.640	16.590
3/B	1050 DIA PRECAST MANHOLE	600 SQ CLASS 'B' LID	17.410	FROM ALL LINES 16.670	16.650
4/B	600 SQ CAST IN-SITU GULLY PIT WITH 200mm THICK WALLS & BASE	600 SQ CLASS 'B' GALV. OR C.I. GRATE & FRAME (HEELSAFE)	17.200	16.910	16.800
5/B	ACO S200K POWER DRAIN (SK2-1 - SK2-18) (SLOPED CHANNEL)	CLASS 'D' INTERCEPT GRATE WITH POWERLOCK	17.300 TO 17.200	17.095	16.910
1/C	450 SQ CAST IN-SITU GULLY PIT WITH 200mm THICK WALLS & BASE. (WATER SEALED)	450 SQ CLASS 'D' GALV. OR C.I. GRATE & FRAME (HEELSAFE)	17.410	-	16.870
2/C	450 SQ CAST IN-SITU GULLY PIT WITH 200mm THICK WALLS & BASE. (WATER SEALED)	450 SQ CLASS 'D' GALV. OR C.I. GRATE & FRAME (HEELSAFE)	17.400	-	16.920
3/C	450 SQ CAST IN-SITU GULLY PIT WITH 200mm THICK WALLS & BASE. (WATER SEALED)	450 SQ CLASS 'D' GALV. OR C.I. GRATE & FRAME (HEELSAFE)	17.410	-	16.990
4/C	450 SQ CAST IN-SITU GULLY PIT WITH 200mm THICK WALLS & BASE. (WATER SEALED)	450 SQ CLASS 'D' GALV. OR C.I. GRATE & FRAME (HEELSAFE)	17.430	-	17.040
OUT/D	2 x ø150 SLOPPING CONCRETE HEADWALL	N/A	16.790	16.430	16.380
1/D	600 SQ CAST IN-SITU GULLY PIT WITH 200mm THICK WALLS & BASE	600 SQ CLASS 'B' GALV. OR C.I. GRATE & FRAME	17.200	16.905	16.900
2/D	ACO S200K POWER DRAIN (SK2-1 - SK2-19) (SLOPED CHANNEL)	CLASS 'D' INTERCEPT GRATE WITH POWERLOCK	17.380 TO 17.200	17.175	16.905
OUT/E	ø150 SLOPPING CONCRETE HEADWALL	N/A	17.450	17.100	17.050
1/E	600 SQ CAST IN-SITU GULLY PIT WITH 200mm THICK WALLS & BASE	600 SQ CLASS 'D' GALV. OR C.I. GRATE & FRAME	17.450	-	17.115
OUT/F	ø150 SLOPPING CONCRETE HEADWALL	N/A	16.940	16.630	16.580
1/F	600 SQ CAST IN-SITU GULLY PIT WITH 200mm THICK WALLS & BASE	600 SQ CLASS 'D' GALV. OR C.I. GRATE & FRAME	16.960	16.660	16.640
2/F ✖	600 SQ CAST IN-SITU GULLY PIT WITH 200mm THICK WALLS & BASE	600 SQ CLASS 'D' GALV. OR C.I. GRATE & FRAME (HEELSAFE)	17.190	-	16.760
OUT/G	ø150 SLOPPING CONCRETE HEADWALL	N/A	17.200	16.870	16.820
1/G	600 SQ CAST IN-SITU GULLY PIT WITH 200mm THICK WALLS & BASE	600 SQ CLASS 'D' GALV. OR C.I. GRATE & FRAME	17.200	-	16.885
OUT/H	ø150 SLOPPING CONCRETE HEADWALL	N/A	16.790	16.440	16.390
1/H	600 SQ CAST IN-SITU GULLY PIT WITH 200mm THICK WALLS & BASE	600 SQ CLASS 'D' GALV. OR C.I. GRATE & FRAME	16.790	16.520	16.460
2/H	ACO S200K POWER DRAIN (SK2-1 - SK2-11) (SLOPED CHANNEL)	CLASS 'D' INTERCEPT GRATE WITH POWERLOCK (BOLTED DOWN)	16.800 TO 16.800	14.750	16.545
OUT/J	ø150 SLOPPING CONCRETE HEADWALL	N/A	16.79	16.420	16.370
1/J	600 SQ CAST IN-SITU GULLY PIT WITH 200mm THICK WALLS & BASE	600 SQ CLASS 'D' GALV. OR C.I. GRATE & FRAME	16.720	16.465	16.440
2/J	ACO S200K POWER DRAIN (SK2-1 - SK2-11) (SLOPED CHANNEL)	CLASS 'D' INTERCEPT GRATE WITH POWERLOCK (BOLTED DOWN)	16.800 TO 16.720	14.750	16.465
1/K	450 SQ CAST IN-SITU GULLY PIT WITH 200mm THICK WALLS & BASE	450 SQ CLASS 'D' GALV. OR C.I. GRATE & FRAME	17.470	-	16.900
OUT/M	ø100 SLOPPING CONCRETE HEADWALL	N/A	16.870	16.550	16.500
1/M	450 SQ CAST IN-SITU GULLY PIT WITH 200mm THICK WALLS & BASE	450 SQ CLASS 'D' GALV. OR C.I. GRATE & FRAME	16.800	16.625	16.600
2/M	ACO S100K POWER DRAIN (SK1-1 - SK1-15) (SLOPED CHANNEL)	CLASS 'D' INTERCEPT GRATE WITH POWERLOCK (BOLTED DOWN)	16.800 TO 16.800	16.700	16.625

✖ GULLY PIT FITTED WITH SPEL STORMSACKS OR APPROVED EQUIVALENT

FOR SURVEY DATA REFER TO CONTOUR AND DETAIL SURVEY DRAWING (REF No R17005-1) PRODUCED BY HOFFMANN SURVEYORS DATED 23.02.2017.



2

ROCKHAMPTON REGIONAL COUNCIL




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Dated: 6 April 2021

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© Copyright TFA Group Pty Ltd.				1	10.10.18	CV		ISSUED FOR TENDER	LS	PROPOSED SERVICE STATION CNR CAPRICORN HWY & HALL ROAD GRACEMERE, QLD.		STORMWATER PIT SCHEDULE		TENDER	
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		<p>Brisbane (Head office) 166 Knapp Street Fortitude Valley QLD 4006 Australia Ph: 61 7 3854 2900 Aust Wide: 1300 794 300 Email: enquiry@tfa.com.au</p> <p>Sydney 8814 5219 Melbourne 9640 0206 Perth 9480 0430</p>										DATE CREATED 02.10.18		A1 SCALE NTS	A3 SCALE NTS
												DRAWING NO 16273-C03		REV 2	

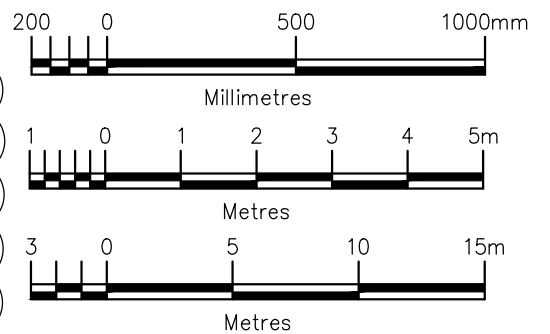
APPROVED PLANS

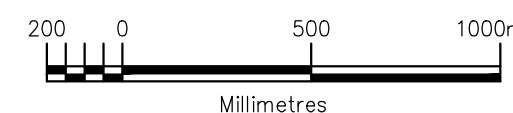
Development Permit No.: D/12-2021

Dated: 6 April 2021



SCALE 1 : 300





1xØ225 RCP CLASS '2' @ 1:188
(U/S IL: 16.00 D/S IL: 15.95)

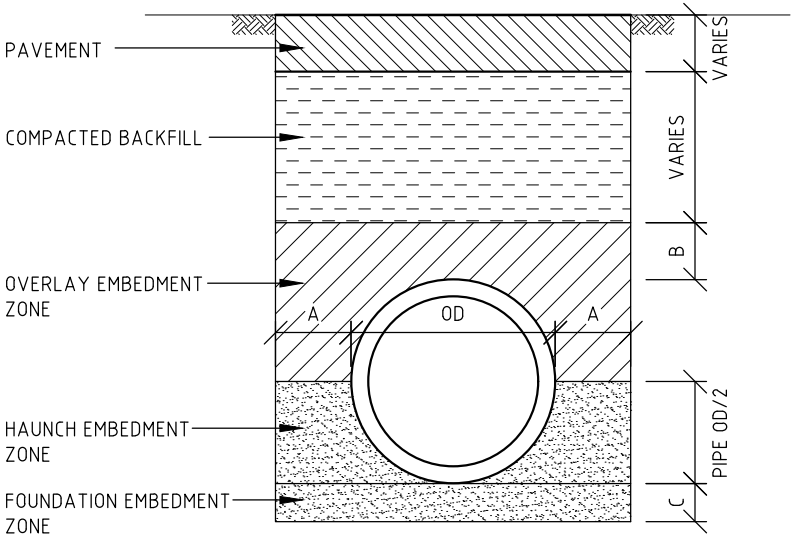
CUSTOMISED MULTI-CELL
PIPE HEADWALL (5 CELL
OF Ø225 PIPES)

1xØ225 RCP CLASS'2'
@ 1:200 (U/S IL: 16.21 D/S IL: 16.185)

4xØ225 RCP CLASS'2'
@ 1:200 (U/S IL: 16.460 D/S IL: 16.435)

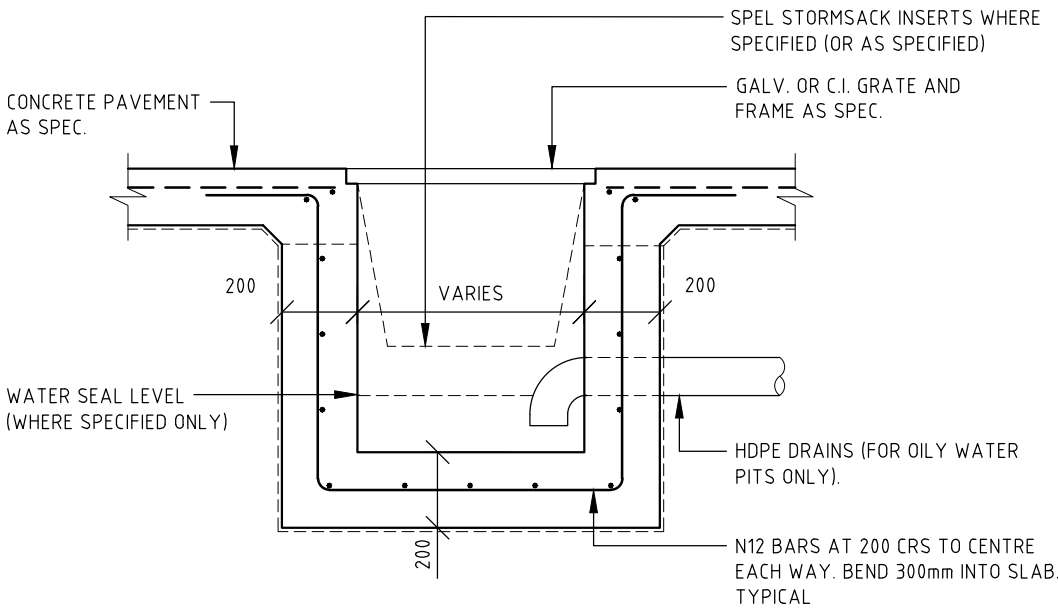
PIPE TRENCHING NOTES:

1. EMBEDMENT ZONE MATERIAL: 5mm OR 10mm SCREENINGS, OR ALTERNATIVELY WASHED SCREENED BEDDING SAND TO GRADING SPECIFIED BELOW TO BED ZONE.
- | SIEVE SIZE (mm) | PASSING (% BY MASS) |
|-----------------|---------------------|
| 19.0 | 100 |
| 2.36 | 40-100 |
| 0.425 | 15-70 |
| 0.075 | 3-30 |
5. BACKFILL TO CONSIST OF APPROVED SUBGRADE REPLACEMENT MATERIAL WITH A MINIMUM CALIFORNIA BEARING RATIO OF 15, PLACED IN 150mm LAYERS AND COMPACTED UNTIL THE DRY DENSITY IS NOT LESS THAN 98% STANDARD MAXIMUM DRY DENSITY.
6. COMPACTED BACKFILL SHALL BE GRANULAR FILL COMPRISING CRUSHED ROCK (75mm MAXIMUM SIZE, NON PLASTIC OPEN GRADED MATERIAL) OR CRUSHER RUN RECYCLED CONCRETE. WHERE APPROVED UNDER SPECIAL CIRCUMSTANCES, STABILISED SAND (1 PART CEMENT TO 12 PARTS SAND BY VOLUME) OR CONTROLLED LOW STRENGTH MATERIAL OR LEAN MIX CONCRETE OR CLASS 3 MATERIAL MAY BE BE PERMITTED.
7. COMPACTED BACKFILL UNDER FOOTPATHS AND BIKEWAYS: EXCAVATED MATERIALS MAY BE USED PROVIDED ADEQUATE COMPACTION CAN BE OBTAINED. ALTERNATIVELY USE GRANULAR FILL OR SAND.
8. WHERE APPROVED, SELECTED MATERIAL FROM EXCAVATIONS SHALL BE PLACED IN 150mm LAYERS & COMPACTED TO A MINIMUM CONSOLIDATION OF 98% STANDARD COMPACTION.
9. INCREASE EXCAVATION LOCALLY AT SPIGOT AND SOCKET JOINTS (RIGID PIPES) TO ENSURE MINIMUM BOTTOM COVER AS SHOWN.



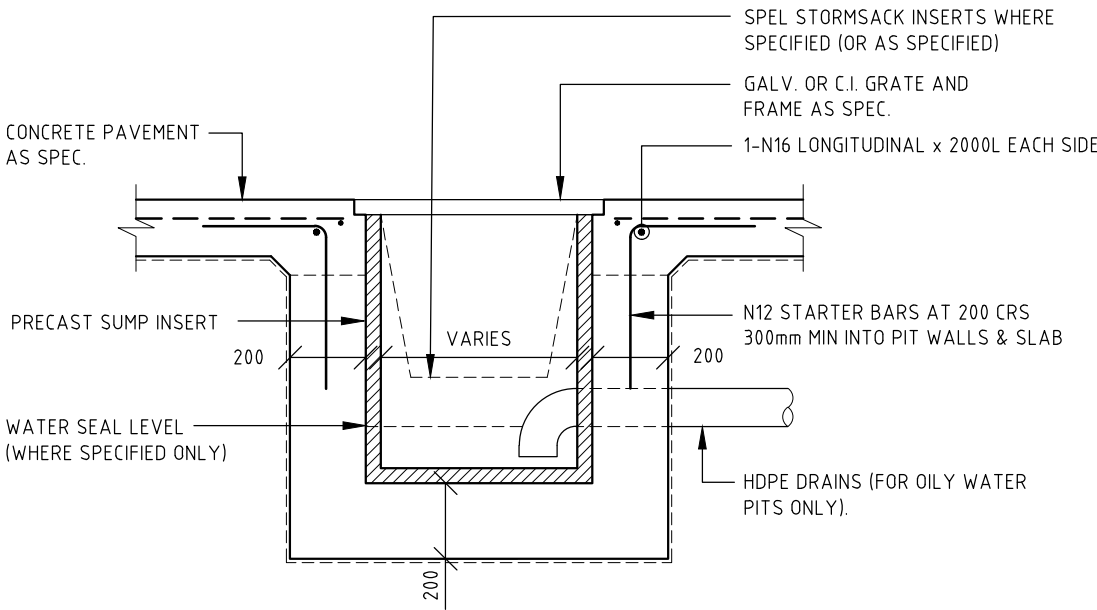
PIPE TRENCH DETAIL (TYPICAL)
NTS

PIPE TRENCH TABLE			
PIPEØ	A	B	C
≥75 ≤150	100	100	75
>150 <300	150	150	100
≥300 ≤450	300	150	100
>450 ≤900	300	150	100
>900 ≤1500	300	200	100
>1500 ≤4000	0.25 x OD	300	150



TYPICAL CAST IN-SITU PIT DETAIL

SCALE 1 : 20
(REFER TO PIT SCHEDULE FOR GULLY PIT SIZES)



ALTERNATIVE PRECAST PIT DETAIL

SCALE 1 : 20
(REFER TO PIT SCHEDULE FOR GULLY PIT SIZES)

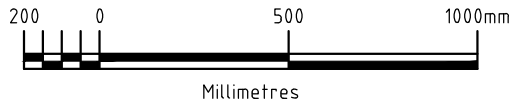
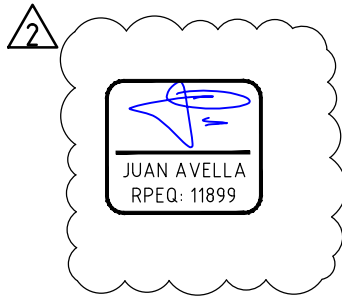
ROCKHAMPTON REGIONAL COUNCIL

APPROVED PLANS

These plans are approved subject to the current conditions of approval associated with

Development Permit No.: D/12-2021

Dated: 6 April 2021



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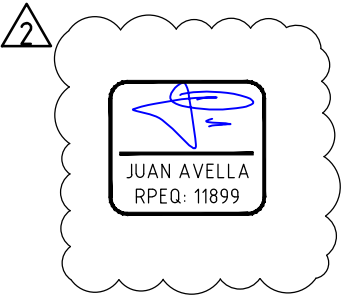
CONCRETE NOTES:

- ALL WORKMANSHIP AND MATERIALS SHALL BE IN ACCORDANCE WITH AS3600.
- CONCRETE QUALITY:

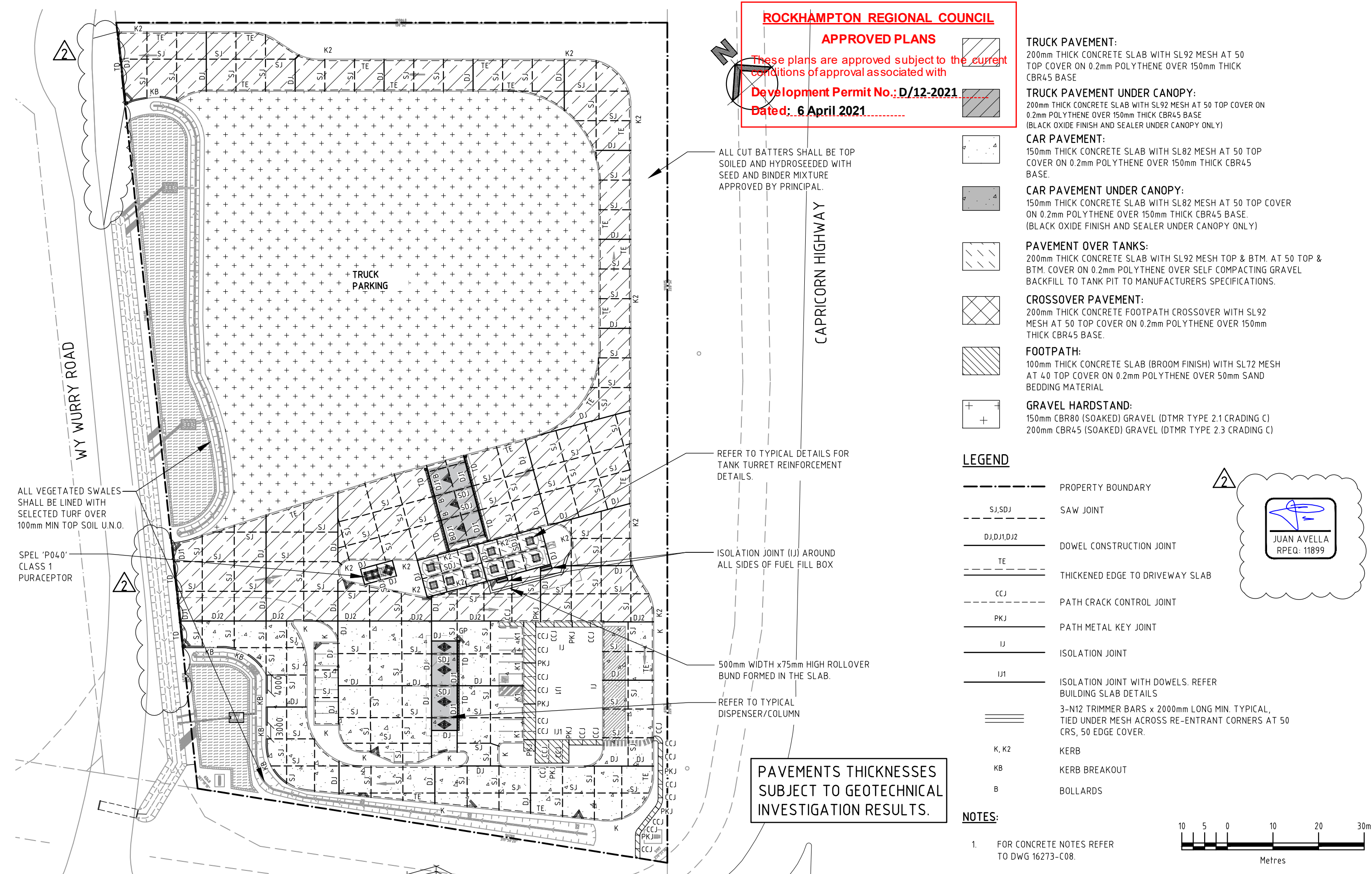
ELEMENT	SLUMP	MAX. SIZE AGG.	CEMENT TYPE	AS 3600 F'C
DRIVEWAY PAVEMENTS, FOOTPATHS, FOOTINGS	80	20	GP	N32
KERBS			GP	N20









- SIZES OF CONCRETE DO NOT INCLUDE THICKNESS OF APPLIED FINISHES.
- CONSTRUCTION JOINTS WHERE NOT SHOWN SHALL BE LOCATED TO THE APPROVAL OF THE ENGINEER.
- NO HOLES OR CHASES OTHER THAN THAT SHOWN ON THE STRUCTURAL DRAWINGS SHALL BE MADE IN CONCRETE MEMBERS WITHOUT THE PRIOR APPROVAL OF THE ENGINEER.
- REINFORCEMENT IS REPRESENTED DIAGRAMMATICALLY. IT IS NOT NECESSARILY SHOWN OF TRUE PROJECTIONS.
- SPLICES IN REINFORCEMENT SHALL BE MADE ONLY ON THE POSITIONS SHOWN. THE WRITTEN APPROVAL OF THE ENGINEER SHALL BE OBTAINED FOR ANY OTHER SPLICES. WHERE LAP LENGTHS ARE NOT SHOWN, THEY SHALL SATISFY THE REQUIREMENTS OF AS3600 OR AS FOLLOWS:-
FABRIC - 1 SQUARE +25mm

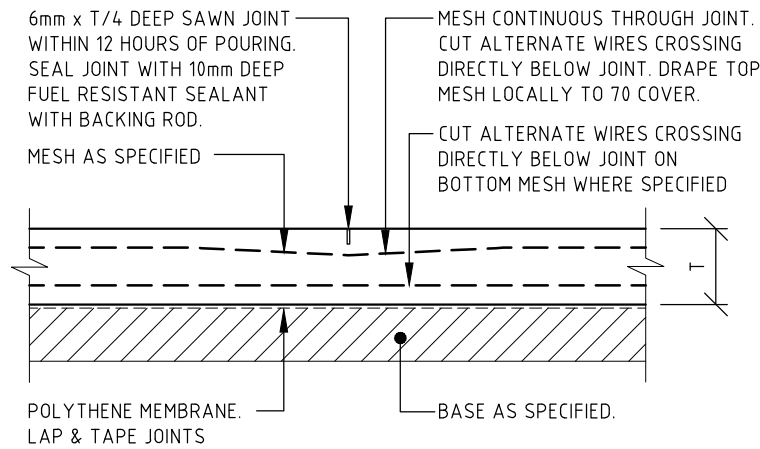
f'c	N12	N16	N20	N28	N32
25	850	1150	1500	2200	2600
32	750	1000	1300	1950	2300
40	650	900	1150	1750	2100



- PIPES OR CONDUITS SHALL NOT BE PLACED WITHIN THE CONCRETE COVER TO REINFORCEMENT WITHOUT THE APPROVAL OF THE ENGINEER.
- ALL STEEL WIRE MESH SHALL BE SUPPLIED IN FLAT SHEETS.
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER 24 HOURS BEFORE REINFORCEMENT IS COMPLETED. THE CONTRACTOR SHALL ALLOW AFTER COMPLETION OF THE REINFORCEMENT, TWO HOURS FOR THE ENGINEERS INSPECTION.
- CONCRETE SHALL NOT BE ORDERED UNTIL REINFORCEMENT IS APPROVED BY THE ENGINEER.
- CONCRETE FINISHING, CURING AND STRIPPING TO BE IN ACCORDANCE WITH THE RELEVANT AUSTRALIAN STANDARDS. CONCRETE TO BE CURED A MINIMUM OF 7 DAYS AFTER POURING BY EITHER APPLYING A SPRAYED MEMBRANE FORMING CURING COMPOUND COMPATIBLE WITH ANY SUBSEQUENT FINISHES TO BE APPLIED TO THE CONCRETE SURFACES, OR COVERING WITH POLYTHENE SHEETING SECURELY HELD IN POSITION.
- CLEAR CONCRETE COVER TO REINFORCEMENT SHALL BE AS INDICATED.
- ALL REINFORCEMENT SHALL BE SUPPORTED ON APPROVED CHAIRS AT A MAXIMUM SPACING OF 1000mm CENTRES IN EACH DIRECTION TO PROVIDE THE CORRECT COVER.
- THE SUPPLY, DELIVERY, SAMPLING AND TESTING OF CONCRETE SHALL BE IN ACCORDANCE WITH AS3600 & AS1379. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ORGANISING APPROPRIATE TESTING BY A NATA CERTIFIED TESTING LABORATORY. RESULTS OF CONCRETE CYLINDER SAMPLE TESTING SHALL BE FORWARDED TO THE ENGINEER FOR REVIEW.

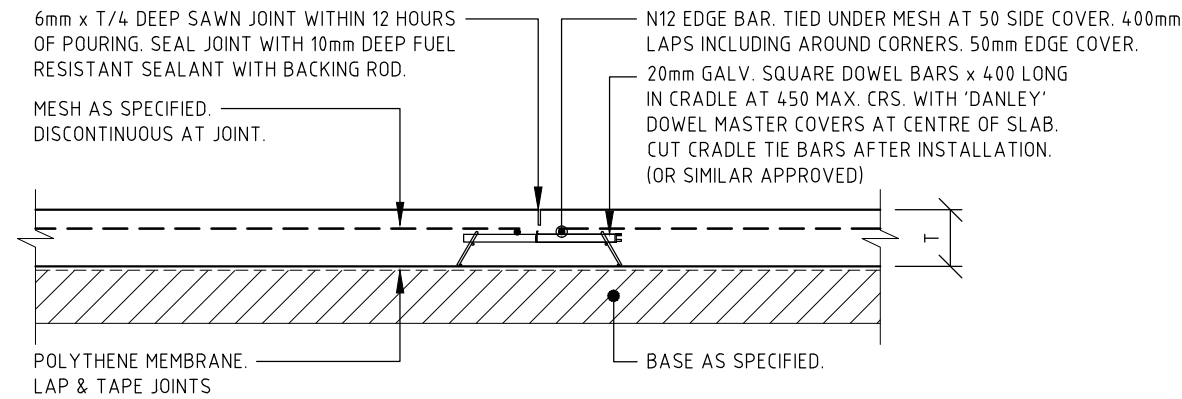


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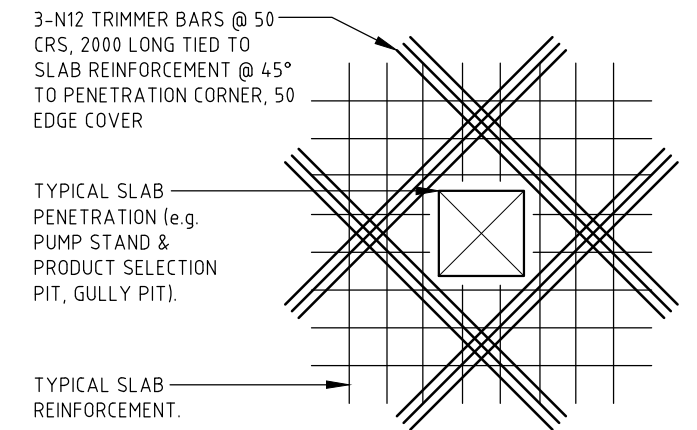
TYPICAL SAW JOINT (SJ)

SCALE 1 : 20



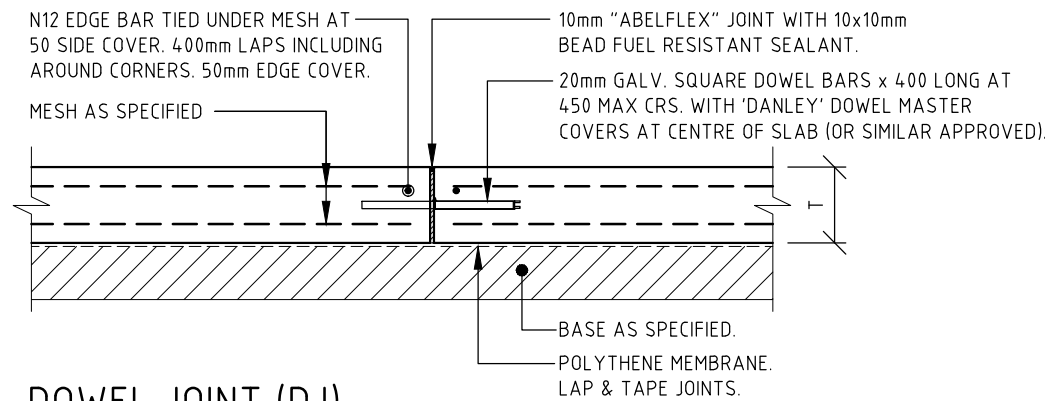
TYPICAL SAW DOWELED JOINT (SDJ)

SCALE 1 : 20



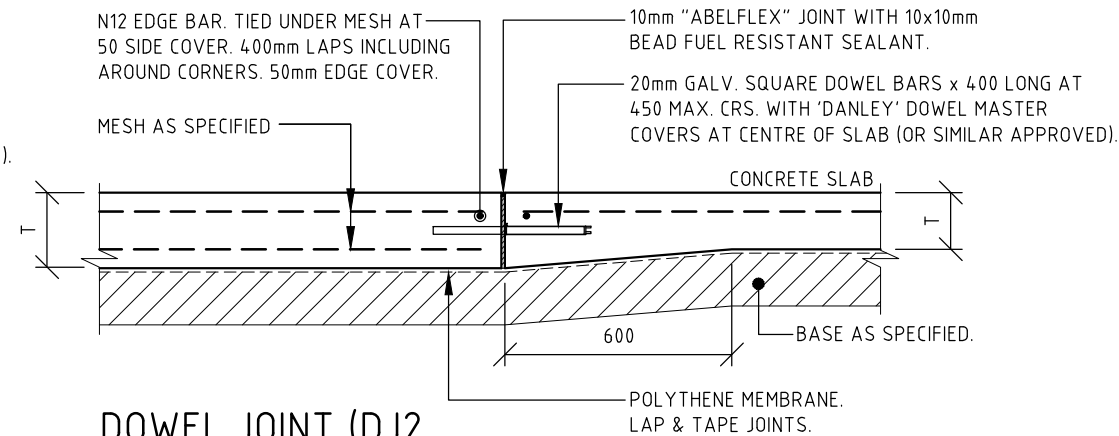
TYPICAL SLAB PENETRATION DETAIL

NOT TO SCALE



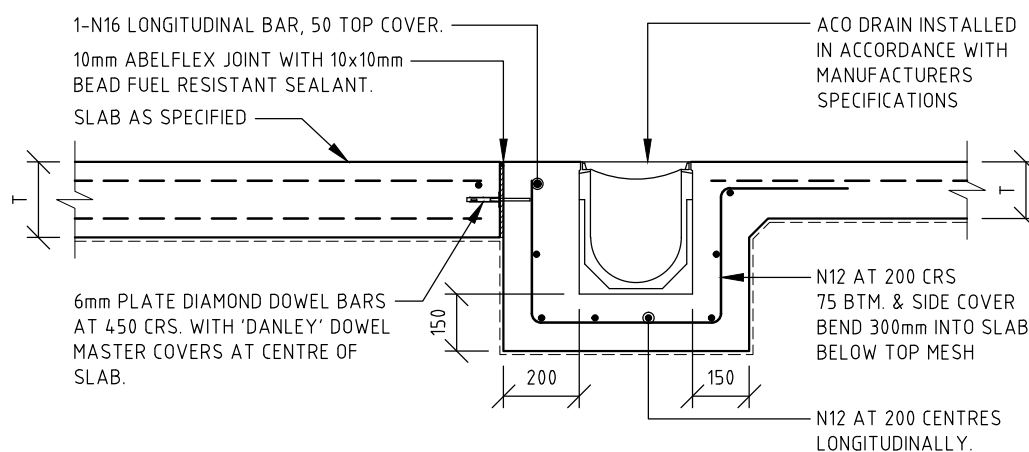
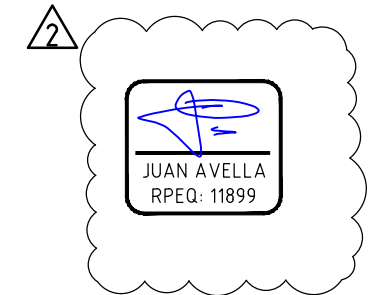
DOWEL JOINT (DJ)

SCALE 1 : 20



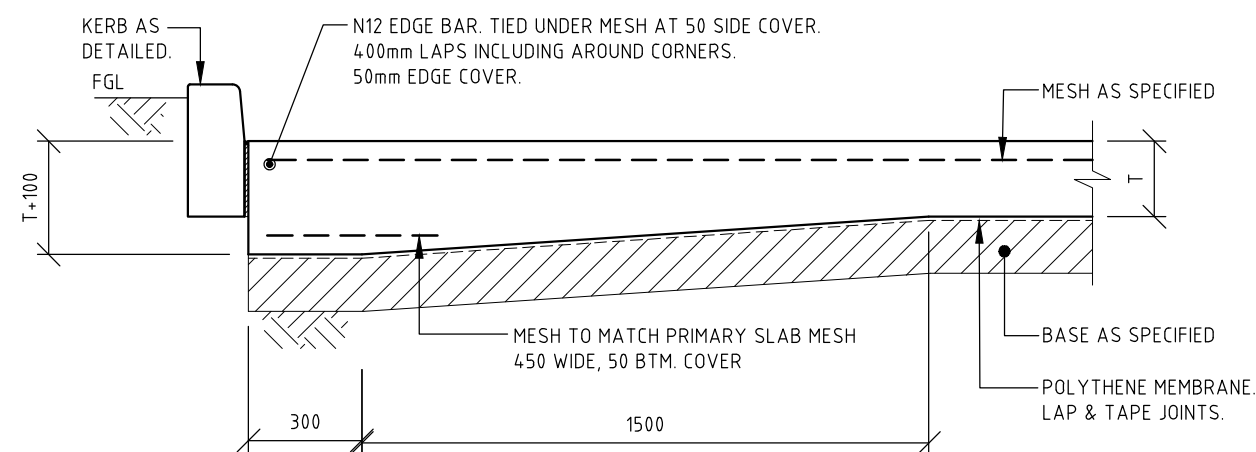
DOWEL JOINT (DJ2)

SCALE 1 : 20



ACO TRENCH DRAIN SECTION WITH DOWEL JOINT (DJ1)

SCALE 1 : 20



TYPICAL THICKENED EDGE (TE)

SCALE 1 : 20

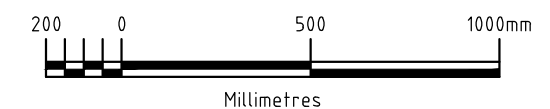
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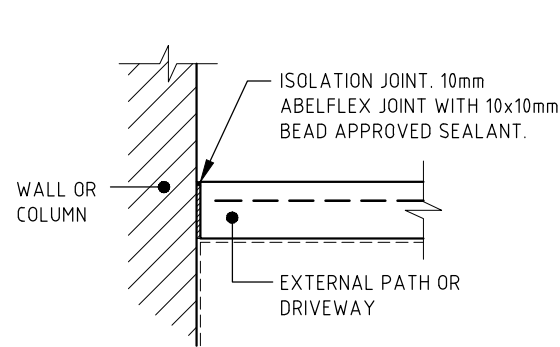
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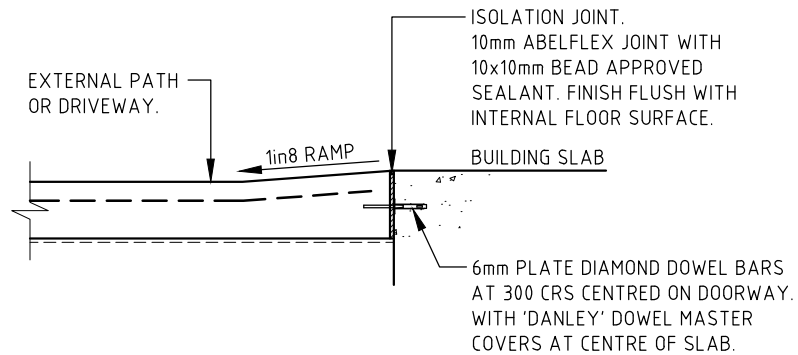


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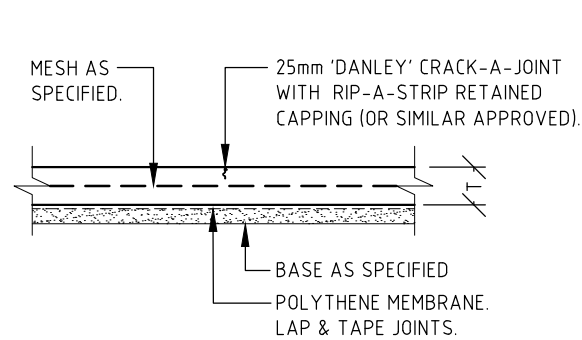
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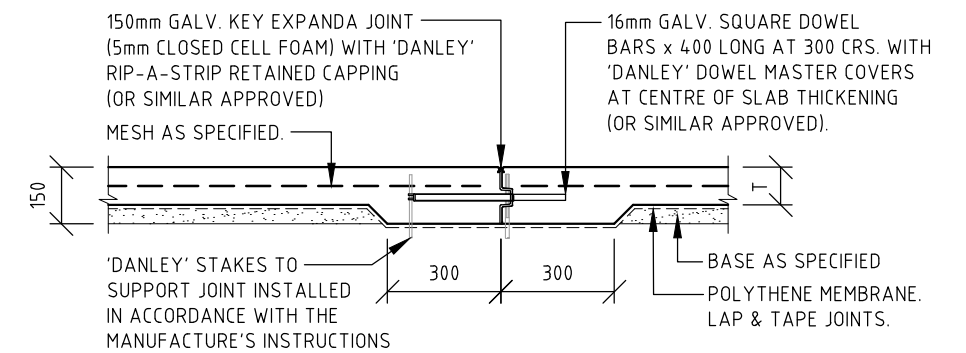
ISOLATION JOINT (IJ1)
(AT DOORWAY ONLY)

SCALE 1 : 20



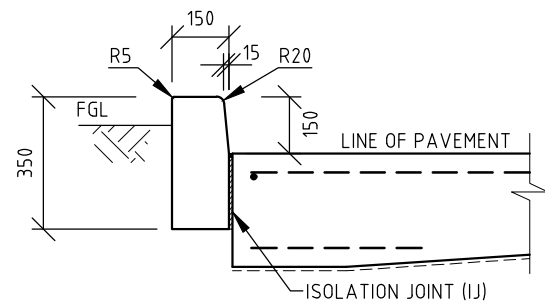
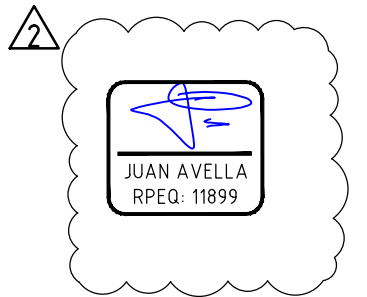
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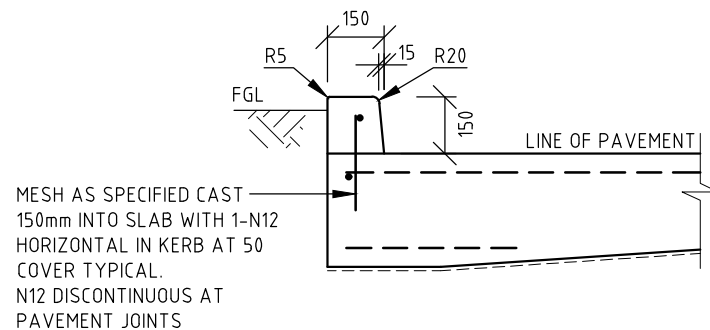
PATH KEY JOINT (PKJ)

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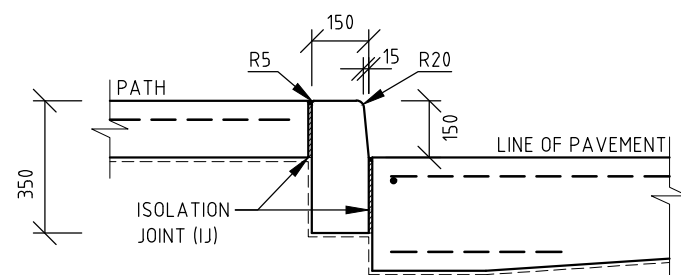
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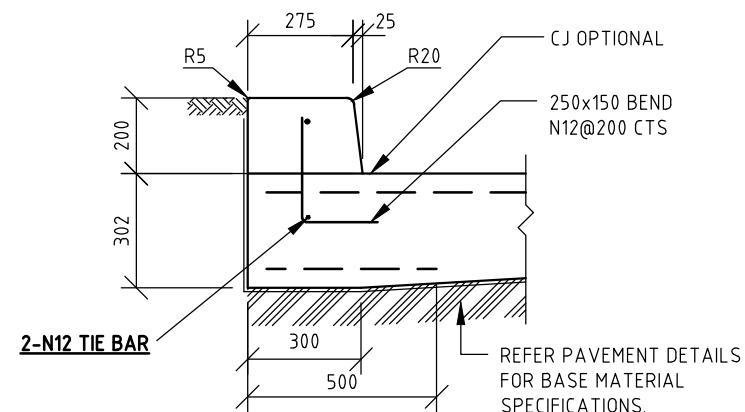
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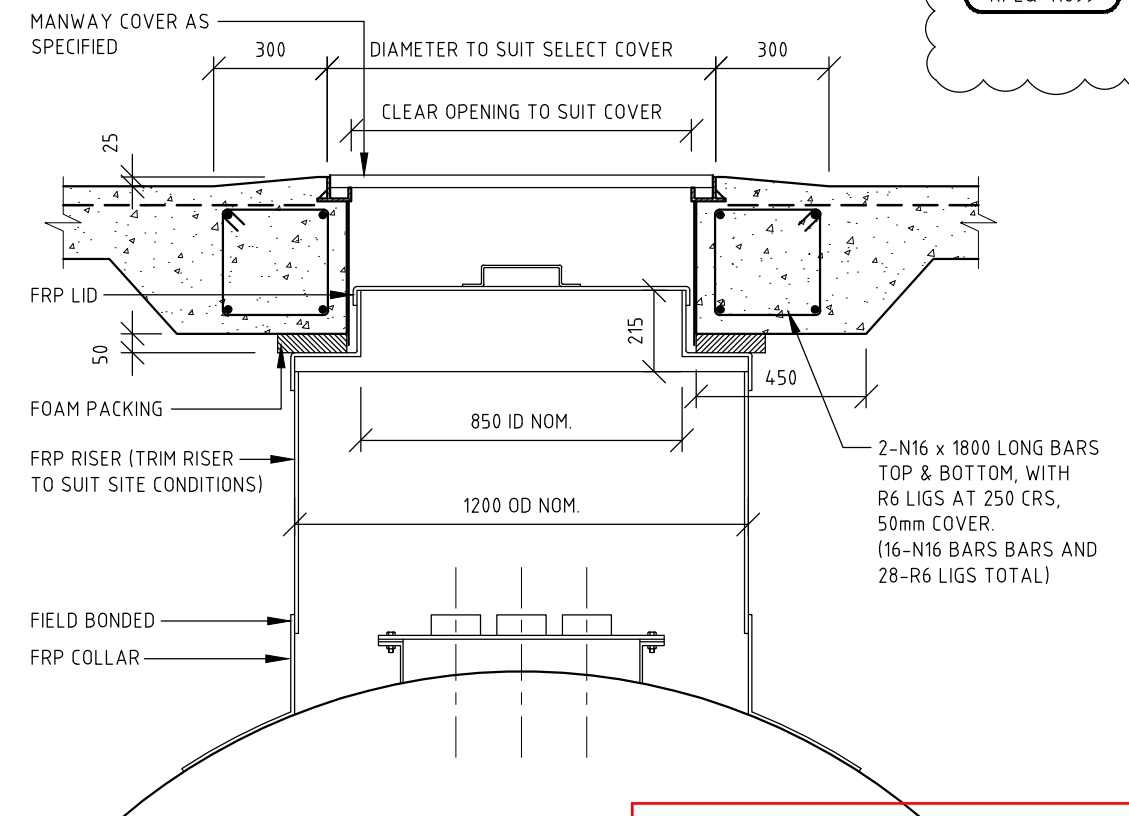
BARRIER KERB DETAIL (K1)

SCALE 1 : 20



TYPICAL INTEGRAL KERB DETAIL (K2)

PROVIDE JOINTS IN KERB TO MATCH JOINTS IN PAVEMENT
SCALE 1 : 20



MANHOLE SECTION TYPICAL

SCALE 1:20

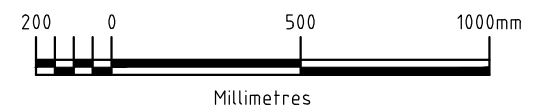
ROCKHAMPTON REGIONAL COUNCIL

APPROVED PLANS

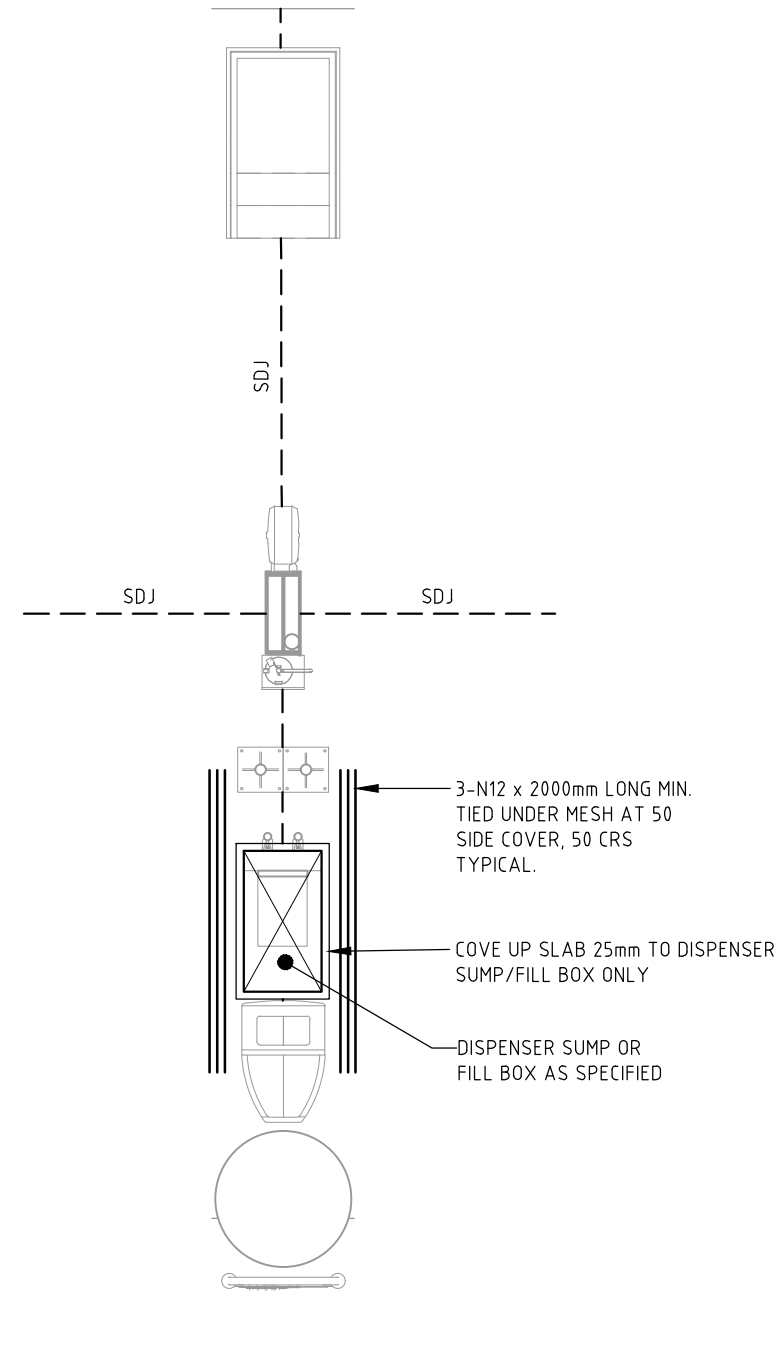
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Dated: 6 April 2021

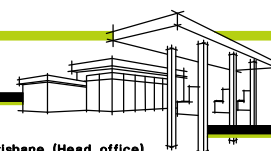
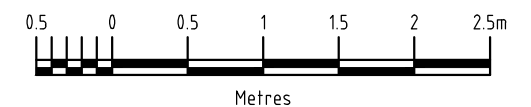


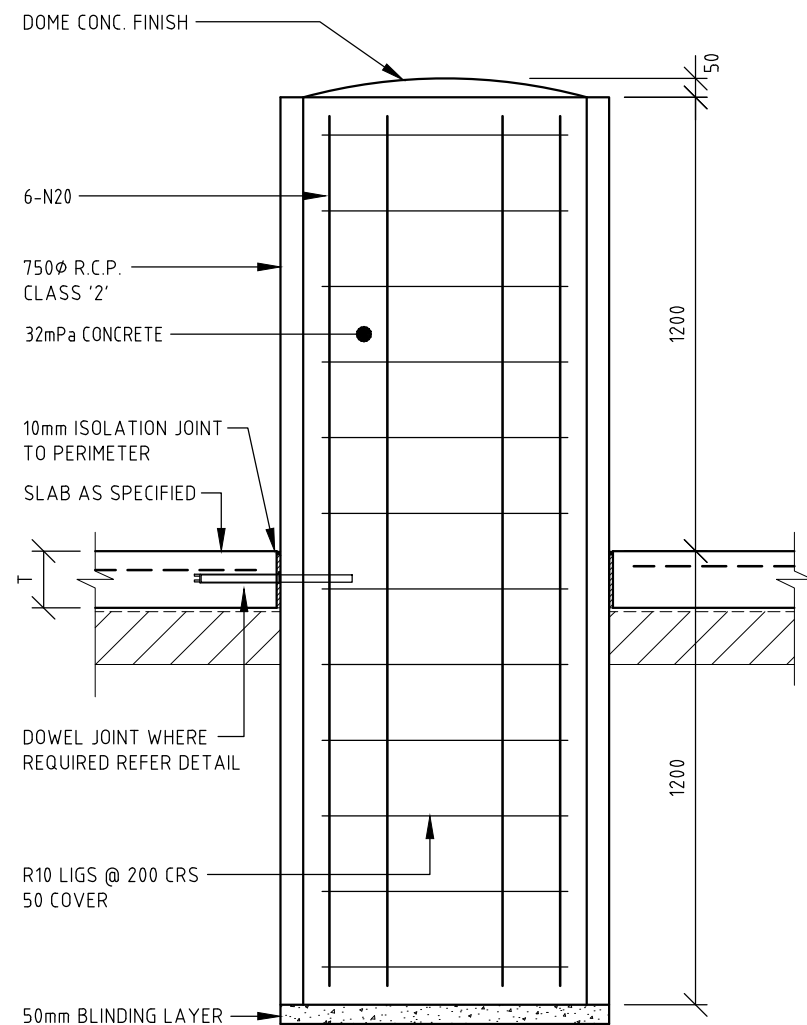
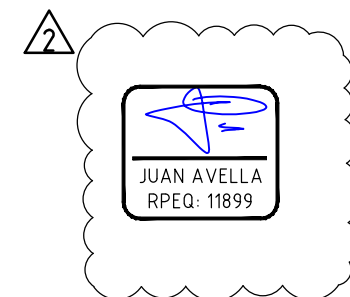
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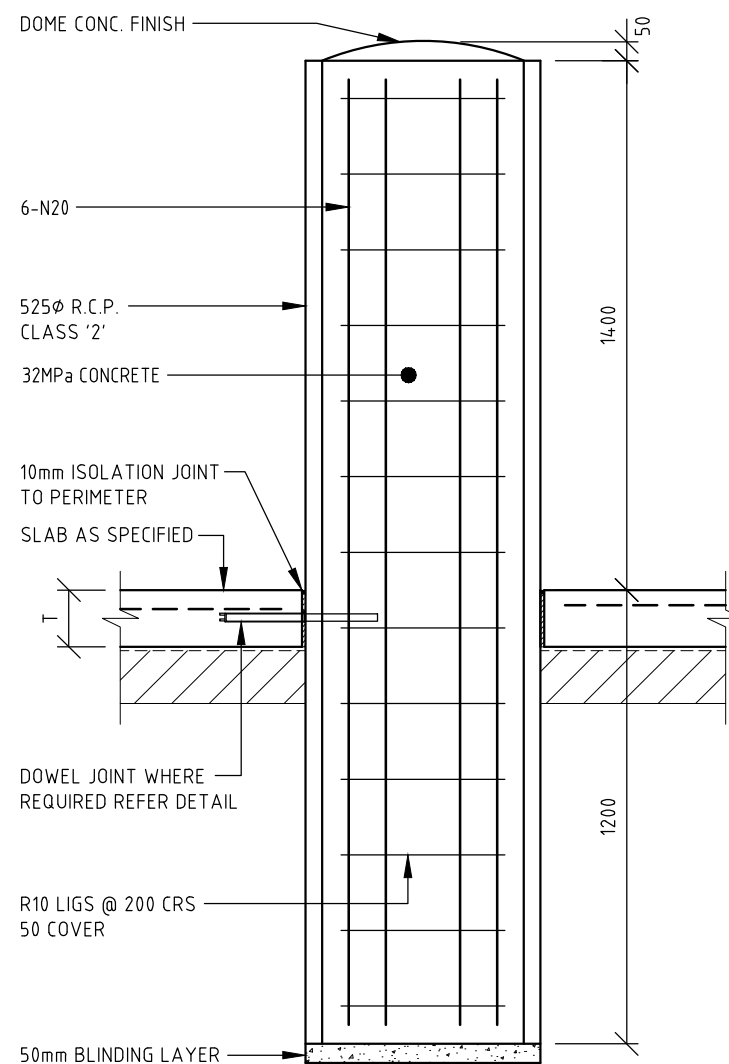
UNDER-TRUCK CANOPY
REINFORCEMENT DETAIL
TYPICAL AT PAVEMENT JOINT
THROUGH SUMP/DISPENSER
SCALE 1:50

Dated: 6 April 2021

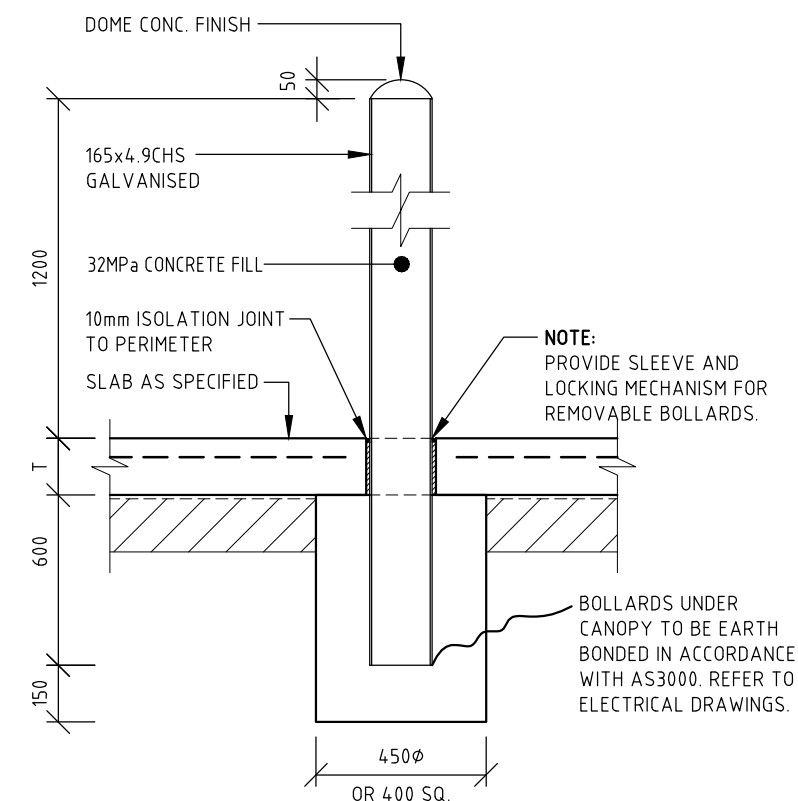




TYPICAL 900 ϕ (NOMINAL) BOLLARD DETAIL
SCALE 1:20



TYPICAL 600 ϕ (NOMINAL) BOLLARD DETAIL
SCALE 1:20



TYPICAL 150 ϕ (NOMINAL) BOLLARD DETAIL
SCALE 1:20

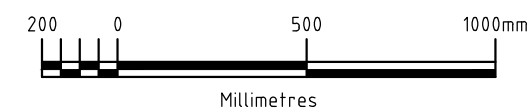
ROCKHAMPTON REGIONAL COUNCIL

APPROVED PLANS

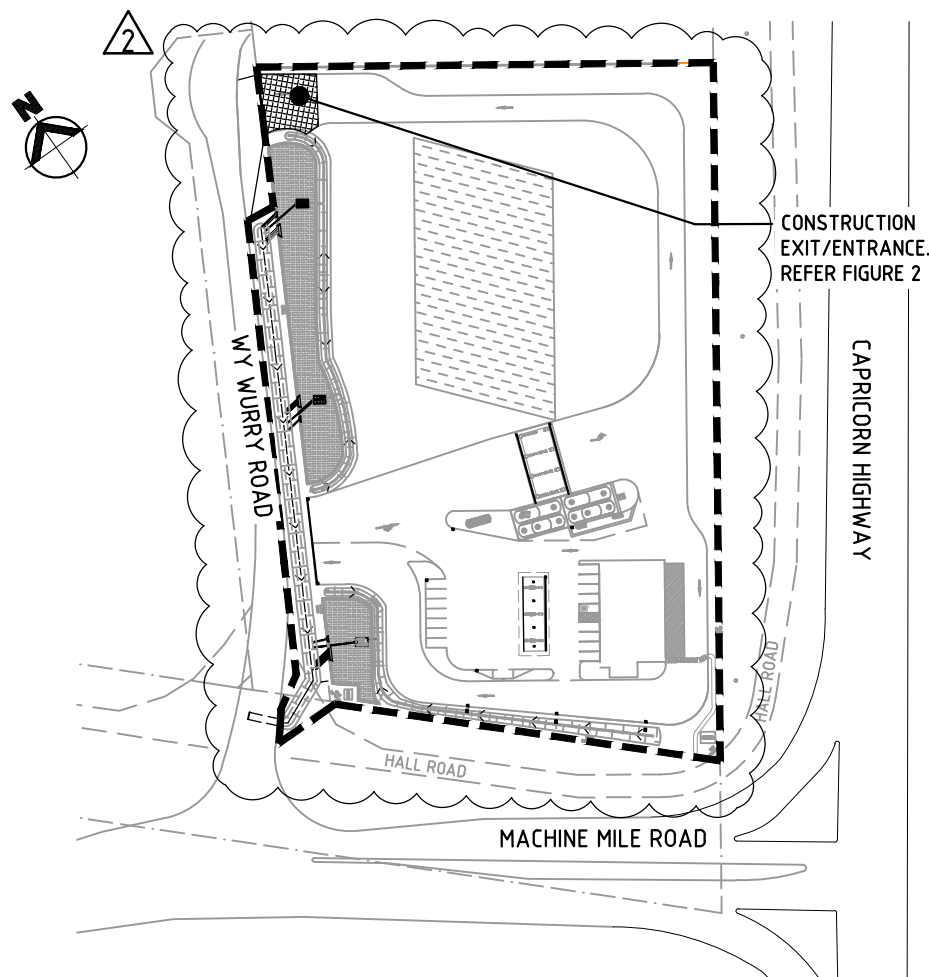
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EROSION AND SEDIMENT CONTROL MEASURES ARE TO BE DESIGNED AND PROVIDED IN ACCORDANCE WITH THE INTERNATIONAL EROSION CONTROL ASSOCIATION AUSTRALASIA - "BEST PRACTICE FOR EROSION AND SEDIMENT CONTROL."

LEGEND



AREA DEFINING EXTENT OF WORKS TO HAVE SEDIMENT AND EROSION CONTROLS INSTALLED

SEDIMENT AND EROSION CONTROL NOTES:

1. ALL THE SEDIMENT & EROSION CONTROL MEASURES TO BE IN ACCORDANCE WITH CAPRICORN MUNICIPAL DEVELOPMENT GUIDELINES (CMDG).

PRIOR TO THE COMMENCEMENT OF CONSTRUCTION:

1. AVOID STRIPPING & EXCAVATING UNTIL READY TO BUILD.
2. INSTALL SEDIMENT FENCES.

SPECIFICATION:

1. SILT FENCE SHALL NOT BE REMOVED UNTIL SITE HAS BEEN PAVED & SURFACED.
2. BUND WALLS SHALL BE LOCATED AROUND ALL PITS & MAINTAINED UNTIL THE CATCHMENT AREA HAS BEEN PAVED.
3. KERB DRAIN EXCLUDER SHALL INCORPORATE TRAFFIC CONTROL BARRICADES IN ACCORDANCE WITH AS1742.3 - 1985, & SHALL NOT BE PLACED UNTIL WORKS ARE BEING CARRIED OUT ON THE FOOTPATH AREA, OR AS OTHERWISE DIRECTED BY ROCKHAMPTON REGIONAL COUNCIL.
4. ALL SEDIMENT TRAPS, EXCLUDERS, BUND WALLS SHALL BE INSPECTED & CLEANED AFTER EACH STORM EVENT. DAMAGED OR CLOGGED BUNDING ARE TO BE REMOVED AND REPLACED.
5. THE BUILDER SHALL CARRY OUT ANY ADDITIONAL WORKS DEEMED NECESSARY AND DIRECTED BY ROCKHAMPTON REGIONAL COUNCIL TO BE CARRIED OUT.
6. THE SEDIMENT CONTROL PLAN SHALL BE IMPLEMENTED PRIOR TO ANY WORKS BEING CARRIED OUT ON SITE.

ROCKHAMPTON REGIONAL COUNCIL

APPROVED PLANS

These plans are approved subject to the current conditions of approval associated with

Development Permit No.: D/12-2021

Dated: 6 April 2021

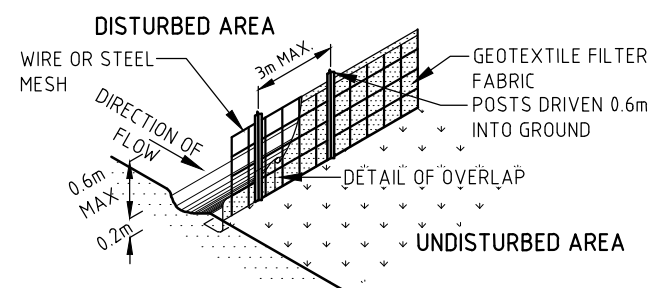


FIG. 1. - SEDIMENT FENCE

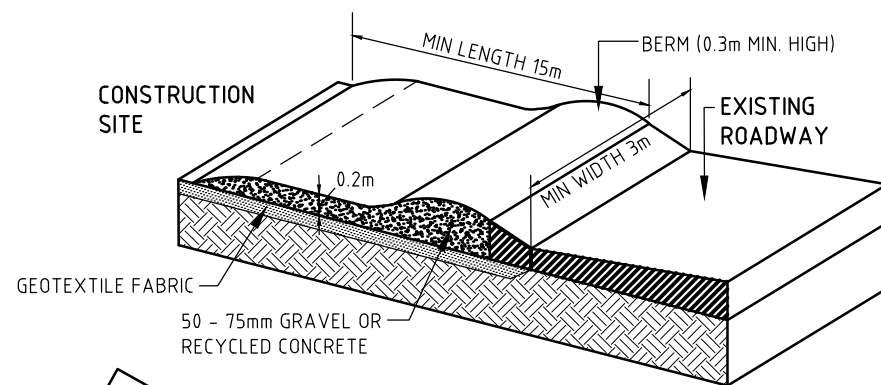


FIG. 2. - CONSTRUCTION EXIT/ENTRY

MAINTENANCE:

1. THE ENTRANCE SHOULD BE MAINTAINED SO THAT IT PREVENTS TYRES FROM TRACKING.
2. DRESSING WITH ADDITIONAL AGGREGATE IF REQUIRED.
3. REGULARLY REMOVE SEDIMENT FROM ROADWAY.

SEDIMENT BARRIERS:

SEDIMENT FENCE

- INSTALL SEDIMENT FENCE(S) ALONG THE LOW SIDE OF THE SITE, AND IDEALLY ALONG A LINE OF CONSTANT LAND LEVEL TO PREVENT THE CONCENTRATION OF STORMWATER RUNOFF. IN AREAS WHERE IT IS EITHER UNDESIRABLE OR IMPRACTICAL TO BURY THE LOWER EDGE OF THE SEDIMENT FENCE, THE LOWER 200mm (MIN) PORTION OF THE FABRIC SHOULD BE PLACED ON THE GROUND UP-SLOPE OF THE FENCE AND BURIED UNDER A 100mm (MIN) LAYER OF AGGREGATE. SEDIMENT FENCES ON BUILDING SITES CAN BE STAPLED TO APPROXIMATELY 40mm SQUARE HARDWOOD POSTS OR WIRE TIED TO STEEL POSTS.

FIELD INLET GULLIES

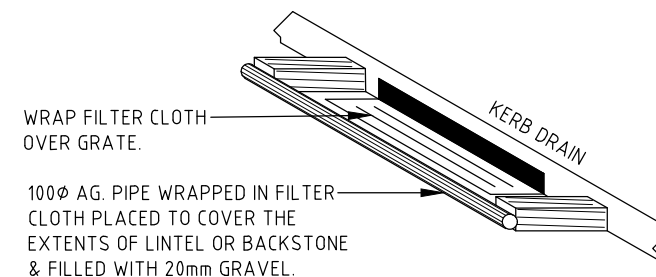
- SEDIMENT CONTROLS FOR STORMWATER INLETS LOCATED WITHIN THE PROPERTY BOUNDARIES MAY CONSIST OF GEOTEXTILE FABRIC PLACED EITHER DIRECTLY OVER THE GRATED INLET OR AROUND THE INLET SUPPORT BY A TIMBER FRAME. FIELD INLET PROTECTION IS NECESSARY WHERE INLETS DRAIN AREAS OF BARE AND UNPROTECTED SOIL. DURING STORMS, PONDING SHOULD BE ALLOWED TO OCCUR AROUND THE STORMWATER INLET TO ASSIST IN THE SETTING OUT OF SEDIMENTS.

PAVEMENT INLET GULLY

- A ROADSIDE INLET BARRIER IS TO BE INSTALLED, SO THAT IT SHOULD NOT BE ALLOWED TO FULLY BLOCK THE INLET STRUCTURE. ON A HILLSIDE, SEDIMENT BARRIERS MAY CONSIST OF A TEMPORARY DAM CONSTRUCTED FROM SAND AND GRAVEL BAGS AT LEAST 4 METRES UP SLOPE FROM THE GULLY INLET.

BUILDING OPERATIONS:

1. ERODABLE MATERIAL MISTAKENLY PLACED WITHIN THE ROAD RESERVE (INCLUDING ACCIDENTAL SPILLAGE AND TRACKING OF SUCH MATERIALS ONTO THE ROAD) THAT CAN NOT BE PREVENTED THROUGH REASONABLE MEANS, MUST BE (a) REMOVED IMMEDIATELY IF RAINFALL IS IMMINENT OR OCCURRING.
- (b) REMOVED PRIOR TO THE END OF THE DAY'S WORK IF RAINFALL NOT EXPECTED. MATERIALS SHOULD BE SWEEPED FROM THE ROAD, NOT WASHED DOWN THE GUTTER.
2. ALL SOLID WASTE SHOULD BE STORED ON SITE IN SUCH A MANNER THAT IT IS PREVENTED FROM LEAVING THE SITE EITHER BY THE ACTION OF WIND OR WATER. SMALLER MATERIALS, SUCH AS LITTER, SHOULD BE CONTAINED IN COVERED BINS OR LITTER TRAPS FORMED ON THREE SIDES BY A GEOTEXTILE WIND BREAK.
3. CONCRETE WASTE WASTED FROM TRUCKS AND MIXERS UNITS SHALL BE CONTAINED ON SITE AND SHALL NOT BE PLACED IN A POSITION WHERE IT COULD REASONABLY BE EXPECTED TO WASH FROM THE SITE AND HARM THE ENVIRONMENT.



KERB INLET PROTECTION

BULK EARTHWORKS:

1. AVOID STRIPPING & EXCAVATING UNTIL READY TO BUILD.
2. CONSTRUCTION OF AN ENTRY/EXIT POINT TO THE SITE SHOULD BE MANAGED SO THAT SEDIMENT IS NOT TRACKED OFF THE SITE.
3. TOP SOIL SHOULD BE STOCKPILED ON SITE FOR LATER USE.
4. WHERE PRACTICABLE MAINTAIN KERB VEGETATION IN A HEALTHY STATE DURING THE CONSTRUCTION PROGRESS.
5. WHEN UP SLOPE WATER IS DIVERTED AROUND A WORK SITE IT IDEALLY SHOULD BE DISCHARGED AS SHEET FLOW THROUGH AN UNDISTURBED AREA BESIDE THE WORKS.

STOCKPILES:

1. STOCKPILES ARE NOT TO BE STORED ON THE FOOTPATH OR THE ROAD RESERVE.
2. WHERE NECESSARY STOCKPILE LOSSES CAN BE MINIMISED WITH THE USE OF COVERS.
3. ALL STOCKPILES AND BUILDING MATERIAL SHOULD BE LOCATED WITHIN SEDIMENT CONTROL ZONE.
4. TO MINIMISE EROSION AND THE LOSS OF SAND AND OIL, STOCKPILES SHOULD BE NOT LOCATED WITHIN AND OVERLAND FLOW PATH. IF IT IS IMPRACTICAL TO AVOID STORMWATER RUNOFF BEING DIRECTED TO A STOCKPILE, THEN A PERIMETER BANK SHOULD BE CONSTRUCTED UP SLOPE OF THE STOCKPILE TO DIRECT RUNOFF IN A CONTROLLED MANNER AROUND THE STOCKPILE.

MAINTENANCE:

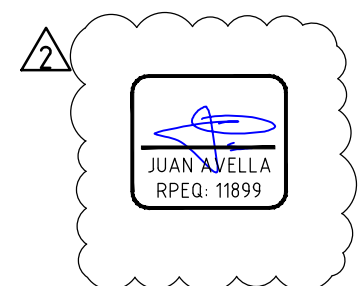
1. SEDIMENT FENCES SHOULD BE REPLACED IF THE FABRIC IS RIPPED OR OTHERWISE DAMAGED. THE MAINTENANCE OF THE SEDIMENT FENCES INCLUDES THE REMOVAL OF SEDIMENT DEPOSITED UP SLOPE OF THE FENCE AND RETRENCHING THE FABRIC WHEN THE FENCE IS 25% FULL.
2. FOLLOWING STORM EVENTS, THE ROAD RESERVE AND ALL SEDIMENT BARRIERS SHOULD BE INSPECTED AND ANY EXCESSIVE RESIDUE SHOULD BE APPROPRIATELY REMOVED.

SITE REHABILITATION:

1. ALL GROUND DISTURBED BY THE CONSTRUCTION ACTIVITY SHOULD BE PROMPTLY AND PROGRESSIVELY STABILISED SO IT CAN NO LONGER ACT AS A SOURCE OF SEDIMENT.

SERVICES TRENCHES:

1. TO AVOID UNNECESSARY SOIL EROSION, SERVICE TRENCHES SHOULD BE BACK FILLED, CAPPED AND COMPACTED TO A LEVEL AT LEAST 75-100mm ABOVE THE ADJOINING GROUND LEVEL.



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						2		06.12.18		LS		NOTES AMENDED TO MAKE REFERENCE TO (CMDG)				CNR CAPRICORN HWY & HALL ROAD GRACEMERE, QLD.		CONTROL PLAN - STAGE 1		APPROVED	
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